

Andrew Brand
Director of Development
Evergreen Housing Development Group LLC
600 University St, Suite 1708
Seattle, WA 98101
206-535-6248 x 101

February 4th, 2015

VIA HAND DELIVERY

Honorable Mayor Dan Holladay
and City Commissioners
City of Oregon City
625 Center Street
Oregon City, OR 97045

RE: AP 14-01/14-02 Beavercreek Road LLC

Dear Mayor Holladay and City Commissioners:

This memo briefly outlines and visualizes the currently approved sewer connection option per the MDNS, sewer option 2, and the option the Applicant, Beavercreek Road LLC, is asking for the option of consideration by City staff, sewer option 1.

Option 2: City Requested Option Per MDNS

Option 2 will require removal and replacement of approximately 500 feet of Meyers Road from the intersection of Beavercreek Road and Meyers Road to the intersection of Beavercreek Road and Emmerson Court. This removal and replacement is required per the City's Pavement Cut Standards. 500 feet of sewer pipe would be installed in a trench going underneath Meyers way and new asphalt would be laid and the roadway would be re-striped upon completion.

Option 1: Option Applicant is seeking to keep open for consideration

Option 1 is not currently approved through the MDNS but the applicant is requesting this option be left open for consideration by the City and allowed to be approved if this connection meets all City standards and will not result in additional costs to the City. Unlike Option 2, Option 1 would only need approximately 150 feet of new pipe and would result in a minimal amount of traffic disruption and public street pavement cuts and replacements. This Option would not require any open trench to cross Beavercreek Road and would utilize an existing public utility easement that is currently covered by a sidewalk connecting Beavercreek Road and the end of Emmerson Court.

Plans on the following pages have visual graphics and notations showing the pavement removal and replacement required by each option and a photo of the public utility easement that would be utilized under Option 1.

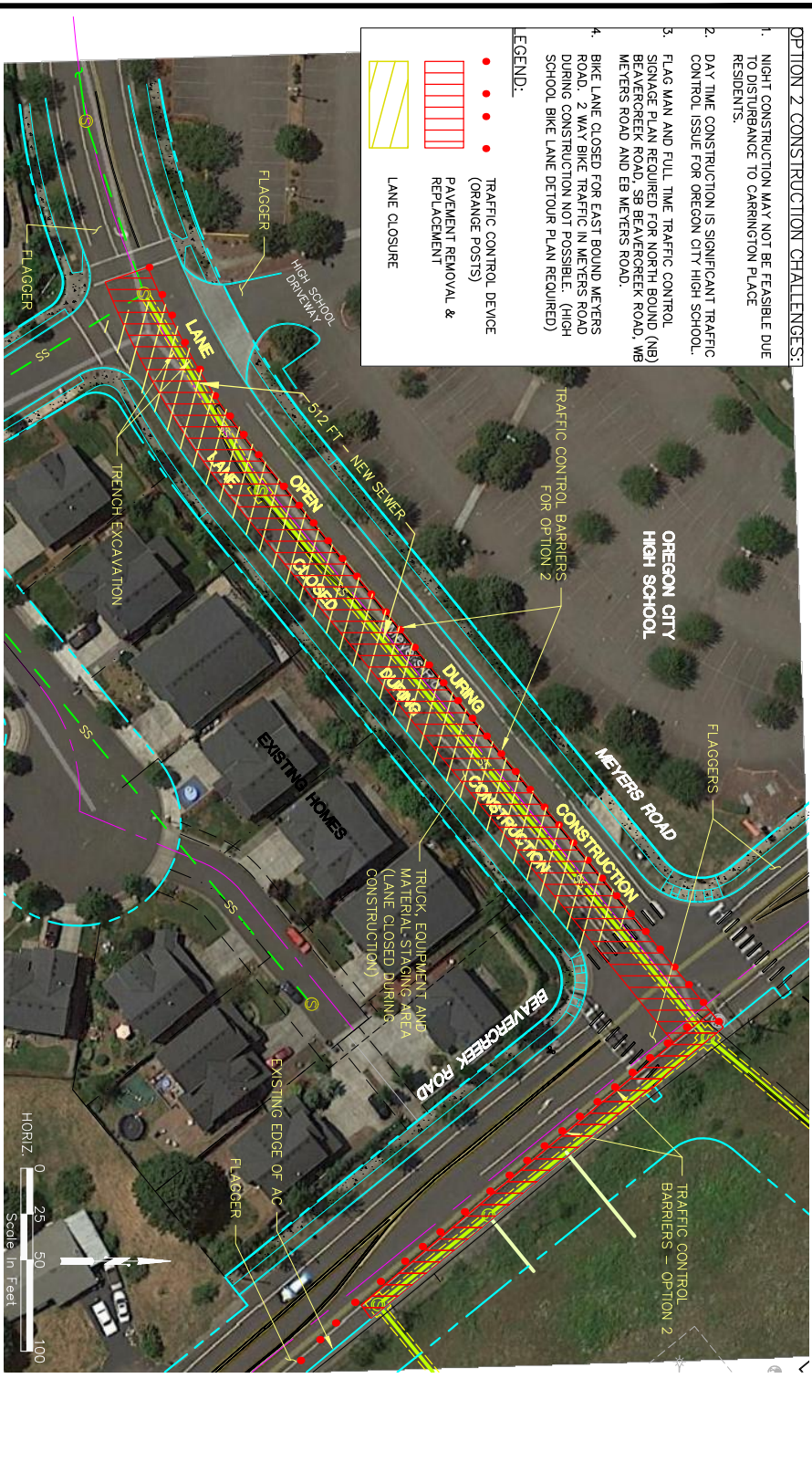
OPTION 2: CURRENT APPROVED OPTION PER MDNS

OPTION 2 CONSTRUCTION CHALLENGES:

1. NING CONSTRUCTION MAY NOT BE FEASIBLE DUE TO DISTURBANCE TO CARRINGTON PLACE RESIDENTS.
2. DAY TIME CONSTRUCTION IS SIGNIFICANT TRAFFIC CONTROL ISSUE FOR OREGON CITY HIGH SCHOOL.
3. FLAG MAN AND FULL TIME TRAFFIC CONTROL SIGNAGE PLAN REQUIRED FOR NORTH ROAD (NB) BEAVERCREEK ROAD, SB BEAVERCREEK ROAD, WB METERS ROAD AND EB METERS ROAD.
4. BIKE LANE CLOSED FOR EAST BOUND METERS ROAD. 2 WAY BIDE TRAFFIC IN METERS ROAD DURING CONSTRUCTION NOT POSSIBLE. (HIGH SCHOOL BIKE LANE DETOUR PLAN REQUIRED)

LEGEND:

- TRAFFIC CONTROL DEVICE
(ORANGE POSTS)
- PAVEMENT REMOVAL &
REPLACEMENT
- LANE CLOSURE



DESIGNED	AAR
DRAWN	AAR
CHECKED	DCP



5000 Meadows Road, Suite 345
Lake Oswego, OR 97035
p. 503.597.3222 | f. 503.597.7655
paceengr.com

DATE	01/29/2015
SCALE	1" = 50'

BEAVERCREEK - TOWNHOME DEVELOPMENT
OPTION 2 - SEWER CONSTRUCTION IN MEYERS ROAD
19896 BEAVERCREEK ROAD
OREGON CITY, OREGON

JOB NUMBER
13869

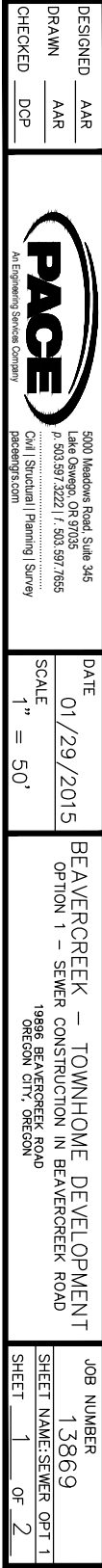
SHEET NAME:SEWER OPT 2

SHEET 2 OF 2

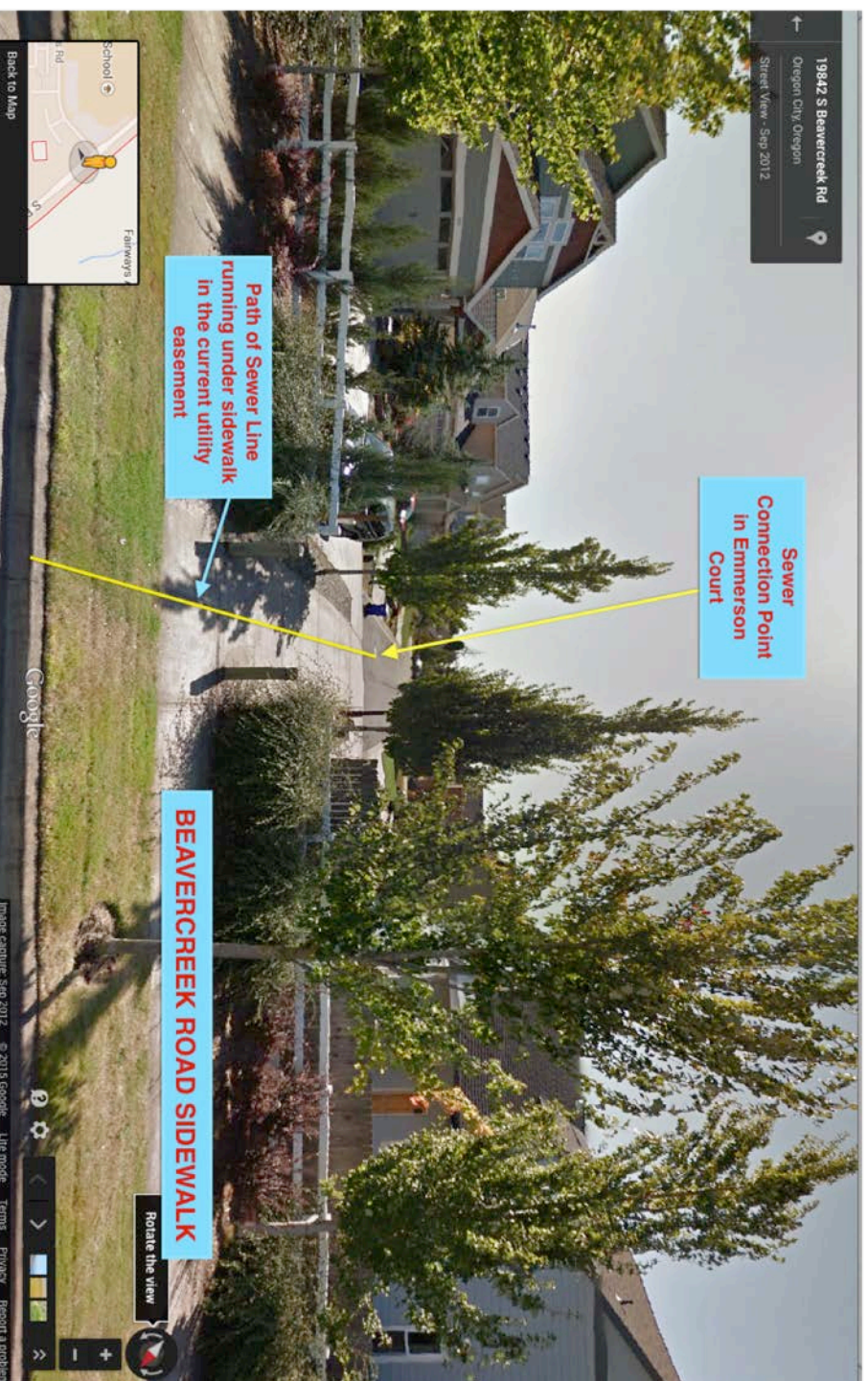
LEGEND:

● ● ● ● TRAFFIC CONTROL DEVICE
 (ORANGE POSTS)

□ □ □ □ □ □ □ □ PAVEMENT REMOVAL &
REPLACEMENT



OPTION 1 : STREET VIEW VISUALIZATION



The City of Oregon City, City Commission
PC 15-151 Beaver Creek Live/Work Apartments
Wednesday, February 4, 2015
Testimony

Good evening... my name is Tammy Stevens and I Chair the Board of The Hamlet of Beaver Creek. We want to thank you for holding the record open. The Hamlet has provided input to your Planners & we appreciate this opportunity to provide additional input to you.

A lot of discussions & testimony last month rightly revolved around transportation, utilities & planning. Our Board's Speaker, Christine Kosinski, has provided you our third letter addressing our concerns regarding these same & additional issues. This evening The Hamlet Board & concerned citizens would like to bring to the forefront our two most pressing concerns... safety & livability.

There are 6,700 citizens in the Hamlet with one main access road. This number does not include our neighbors in Clarkes, Molalla & Mulino who utilize BC Road to access the metro area daily. Granted, we could utilize the narrow & dangerous Henrici Road to Redland or battle the congestion of Hwy 213, but Beaver Creek is named "the world's largest cul-de-sac" for a reason... Beaver Creek Road. What the humble Hamlet citizen living south of Oregon City thinks about is... getting to & from home without unreasonable congestion & feeling confident emergency services can reach their family in a time of need. The Clackamas County Fire District #1 Suburban community response time is 7 minutes & 37 seconds. Their rural response time is 14 minutes 38 seconds. Would you like your family on the other end of them having congested rural access?



The discussions and plans of development along Beaver Creek Road without a thorough, long term plan to include transportation, utilities, growth, concurrency, safety, livability and funding reminds me of another just such a primary/life line road... Sunnyside. When Happy Valley developed & developed & developed, the County was forced to step in & fix their transportation problem to the tune of tens of millions of dollars. Now, Happy Valley is happy with their two new roads. Beaver Creek Road (like Sunnyside Road) is under the jurisdiction of the County. And, no matter what growth happens on Beaver Creek Road, the County no longer has any money to fix a congestion problem like they did on Sunnyside. In fact, the County is seriously struggling to find the money just to maintain their 1,400 miles of roads.

So, if the City, County, State & Federal governments cannot fix the results of unplanned, unfunded growth along Beaver Creek Road, who suffers? Do the OC citizens living north of Beaver Creek suffer? Do the employees of the County & City north of Beaver Creek suffer? Do the businesses & residents right on Beaver Creek Road suffer? NO! The small businesses & residents of The Hamlet of Beaver Creek living on Wilson, Ferguson, Steiner, Yeoman, Carus, Spangler, Upper & Lower Highland, Schuebel, Larkin, Buckner Creek, Gard, Kamrath, Leland, Lammer, Levi, Ivel & McBurney Roads do.

And why? Unplanned spot development of 9.7 acres of apartments, an 8 acre bus facility, future development off Maple Lane, an extensive Corridor & a potential industrial enterprise zone all of which feeds into a failed intersection at Hwy 213 & Beaver Creek Road needing a \$40MM overpass.



We sincerely understand the value of a healthy, growing community with jobs, economic growth & a future. What we don't understand & are asking each of you this evening to consider is whether you have the cart before the horse. Berry Hill & Haggens shopping centers struggle daily to secure successful, long standing businesses. How can you attract industrial or commercial businesses even further out with limited transportation access?

And, finally, I want to leave you with two additional messages:

The first, I sat on the Clackamas County Planning Commission for nearly a decade & numerous County committees & if my Planners said to me... we're going to build a parallel road & install a huge sewer system & expand our water system, my reaction would have mirrored those of Commissioners Shaw & Pauli... "really?" I ask you to ask how you build roads over territory riddled with landslides & sewer & water systems with unavailable hundreds of millions of dollars? The fundamental theory of immediate economic development is urban renewal & increased density inside the boundary where all services exist, not expanding and developing a rural area with nonexistent services & topography challenges.

And, finally... as always, we appreciate this opportunity to provide citizen input & share concerns. As an agent of the County & your neighbor, we look forward to working together for the success, safety & livability of our citizens & businesses. We certainly don't want to obstruct the economic growth & success of Oregon City, however, it cannot be at the expense of Beaver Creek's safety & livability.

HOMES BUILT & FINISHED IN HOLLY LANE AREA

<u>NUMBER OF HOMES (APPROX)</u>		<u>ADDED ADT'S PER DAY</u>
WALNUT GROVE	250 HOMES	2,500
CRABTREE TERRACE	185	1,850
NEW HOMES – THAYER	84	840
THAYER ESTATES	200	2,000
THAYER IN-FILL	11	110
IN-FILL IN THE AREA	100	1,000

PROPOSED DEVELOPMENT, MANY WILL USE THE HOLLY LANE EXTENSION

BEAVERCREEK/MEYERS	121 APTS	726
BEAVERCREEK/MEYERS	59 LIVE/WORK	500+
BEAVERCREEK CONCEPT	1,300 HOMES	13,000 **
BEAVERCREEK CONCEPT	COMMERCIAL	30,000 **
PARK PLACE PLAN	1,459 HOMES	20,000 **

Development proposed Old Bus Barn Maplelane/Beavercreek	At least 1-2,000 trips per day
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Possible Enterprise Zone	At least 1-2,000 trips per day
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GRAND TOTAL.....ABOUT 75,000 NEW TRIPS PER DAY*

*MANY OF THESE DAILY TRIPS WILL UTILIZE HOLLY LANE AS DESIGNED BY THE CITY'S TSP, TO PUSH TRAFFIC INLAND TO PREVENT OVER SATURATION OF THE ALREADY FAILING BEAVERCREEK ROAD. THE CITY PLAN IS TO PUT AS MUCH TRAFFIC DOWN HOLLY LANE, THE ONE AND ONLY NORTH/SOUTH CONNECTOR IN THIS ENTIRE AREA.

** ADT's Per Concept Plan

Traffic coming from the proposed Central Road (or Holly Ln extension) includes the Industrial zone, traffic coming from Henrici, Loder, Glen Oak, Caufield. Traffic coming across Beavercreek from the Linn, Leland, Meyers Corridor and accessing the Holly Ln Extension, all of this will add thousands more ADT'S per day.

Oregon City Commission
Meeting of February 4th, 2015

RE: Agenda Number 6 (b) PC 15-151 Planning File SP 14-01 Beavercreek Road Live/Work
Apartments (Planning Files AP 14-01 and AP 14-02

Testimony of: Christine Kosinski, Unincorporated Clackamas County

At the January 21st, 2015 hearing, Commissioners Shaw and Pauli asked about roads raising concerns that Beavercreek Road would only be a three lane road, questioning capacity.

The first handout you have is a page from the Oregon City TSP, indicating their plan to heavily use Holly Lane to carry traffic being pushed into an interior central road, Follow the dark black line on the Map, from Henrici to Thayer to Maplelane to Holly and up to Holcomb, this shows the Freeway the City proposes for Holly Lane, all to save Beavercreek Road which is inadequate to carry all the traffic being proposed for this area.

The second handout lists approximate ADT's per day that the City proposes for this hilltop area, trips per day that have only ONE Major Arterial and ONE little North/South Connector that is riddled with landslides.

Homes on Holly sit within a few feet of the road with over 100 driveways directly accessing the road. The City plans to widen Holly where traffic will zoom right in front of homeowner's living room windows. Ancient landslides lining the street can be easily re-activated by excavating or by heavy vibrations. The City, has previously been made aware, that Landslide Insurance is no longer available to the people. We have asked the City numerous times “ if you excavate on Holly and re-activate landslides, who pays the property damages? We have never received an answer.

So there you are, the City is planning huge development with ONLY ONE MAJOR ARTERIAL AND ONE VERY SMALL NORTH/SOUTH CONNECTOR FULL OF LANDSLIDES AND AN IRRATIONAL TRANSPORTATION PLAN! There is simply NO INFRASTRUCTURE, NO ROADS AND NO FINANCIAL PLAN to support either the subject land use or the plethora of development you are proposing for this hilltop area.



ENTERED INTO THE RECORD
DATE RECEIVED: 2/4/15
SUBMITTED BY: Johnny Stevens
SUBJECT: Item 6b

Oregon City Commission
Dan Holladay, Mayor
Carol Pauli, Commissioner
Rocky Smith, Commissioner
Brian Shaw, Commissioner
Daphne Wuest, Commissioner
David Frasher, City Manager
625 Center Street
Oregon City, OR 97045

February 4, 2015

RE: Agenda Item 6 b PC 15-151 – Planning File SP 14-01 Beaver Creek Road Live/Work Apartments
(Planning Files AP 14-01 and AP 14-02)

Both at The Hamlet of Beaver Creek Board Meeting, held on Thursday, January 22nd, 2015, and The Hamlet of Beaver Creek Town Hall meeting, held on January 28th, 2015, the impacts and concerns of the Beaver Creek Community regarding the above subject land use application were discussed.

The Board voted unanimously to send a letter to Oregon City listing the impacts and concerns which this development will place upon their community. We ask that our letter be included into the record for the subject land use proposal.

Since the Beaver Creek Community learned of the proposed development at Beaver Creek and Meyers, huge concerns have been brought forward by the people, and these are:

TRANSPORTATION

Beaver Creek Road is the ONE and ONLY NORTH ACCESS for the people of Beaver Creek. It provides a lifeline to vital services necessary to the community. Services such as lifesaving Emergency Police, Fire and Ambulance and bus services to transport their children to and from Oregon City schools, Beaver Creek is the only road which gives our people access to the transportation system, ie, Hwy 213, I-205 and I-5. It is the only road leading to local area grocers, retail shops, physicians, dentists and many other vital daily services. Without having full access to Beaver Creek Road, the community of Beaver Creek simply could not continue to exist. In addition to the 6700 people from Beaver Creek who use Beaver Creek Road on a daily basis, it is an important access road for the people living in Clarkes, Molalla and Mulino.



SAFETY

Response times for emergencies is already marginal at times and will become intolerable. This land use will create a bottleneck, hemming in Beaver Creek Road with no ability to widen for future development. A recent accident occurred at Beaver Creek and Glen Oak, gridlocking traffic for more than 2 hours and drastically affecting the deliverance of emergency services. Citizens, parents and caregivers were unable to reach children and the elderly in need of help.

INFRASTRUCTURE

Water remains an outstanding issue. City water pressure is too low to service the development. It may be possible to deliver water through an IGA agreement with Clackamas River Water, however, there is no signed agreement as of this date. This would not be a permanent solution, but only a temporary fix to a much larger problem.

Sewer services are marginal. Current plans are to hook into Glen Oak, however, if this is to be attempted, Keller Associates (consultant) recommends that the manholes with only two feet of cover be equipped with bolted down, sealed manholes if these are currently not in place. A Citizen from the Glen Oak area testified before the City Commission on January 21st, 2015, that there have already been overflowing sewers on Glen Oak, and has concerns that further development will only exacerbate this difficult situation.

There are virtually NO ROADS to serve this entire hilltop area of Oregon City. Transportation and traffic are overwhelming when considering the heavy amount of development being proposed by the City, as listed below:

1. 121 Unit Apartments and 59 Live-Work units on 9.7 acres located Beaver Creek & Meyers.
2. New 8 acre Bus Barn/Maintenance Facility located off Meyers Road.
3. Possible Enterprise Zone (about 1 mile in size) located along Beaver Creek Rd by CCC.
4. Development at the Old Bus Barn located off Maple Lane, Beaver Creek and Hwy 213.
5. Development of the Linn-Leland-Meyers Corridor.
6. The City will soon re-propose the Beaver Creek Concept Plan, previously about 1300 homes with a large Commercial Center.
7. City development will not just stop here, development will continue down Henrici, on Beaver Creek and throughout this hilltop area, all with NO needed roads.

Just how do you develop all of the above, with only ONE MAJOR ARTERIAL to serve this plethora of new traffic, and an Arterial that the City states will only be two, or possibly three, lanes wide from Beaver Creek & Meyers to the College, creating yet another large bottleneck?

NO MASTER PLAN

Development being considered by the City, as listed above, is simply staggering, and yet NO MASTER PLAN is being proposed. There are virtually NO roads, NO adequate water, sewer or stormwater. Without a Master Plan, there is NO CONCURRENCY and NO guaranteed financial plan in place to successfully develop assuring safe and livable neighborhoods.



STAKEHOLDERS

Members of the community wondered why Oregon City did not include The Hamlet of Beavercreek as a stakeholder in planning this development. This was a large concern we brought to your attention in our letter to you dated August 11th, 2014. We pointed out that the Caufield Neighborhood had been invited to meet regarding this proposed land use, however, The Hamlet of Beavercreek (population much larger than that of Caufield neighborhood) received no such invitation, even though Beavercreek is the one community that will be the most impacted by this development. We also draw your attention to the fact, that in 2010, the Beavercreek community came to Oregon City requesting to be recognized by the City on any and all land use applications that will have great effects upon them. We raised serious concerns that services to our community must continue to be available to our people on a continual daily basis. These would be services such as an open transportation corridor down Beavercreek Road, allowing us easy access into and out of our community, the ability to get to and from work, to transport children to and from school, sporting events, doctors, dentists and allowing extremely important emergency services to quickly reach our people in times of great need.

When considering the serious lack of infrastructure, especially that of servicing this large area with only ONE ROAD, The Hamlet of Beavercreek finds that it cannot support this proposed development without a Master Plan in place to ensure that all necessary infrastructure be in place, with an approved financial plan, prior to any development being undertaken.

Respectfully submitted by

Christine Kosinski
Corresponding Secretary
The Hamlet of Beavercreek
Cor.Sec@Beavercreek.org
503-656-1029

Mr. Pete Walter
Community Development and Planning Dept.
City of Oregon City
221 Molalla Ave. Suite 200
Oregon City, OR 97045

2015 JAN 30 AM 9:11

RECEIVED
CITY OF OREGON CITY

January 28, 2015

Dear Mr. Walter,

Prior to the Conditional Approval of The Beavercreek Road Apartments project (SP 14-01) the Caufield Neighborhood Association submitted comments which addressed several concerns of local residents. These included worries about the proximity of the airstrip, lack of industrial use and vehicle traffic control. These were all addressed in the Conditions of Approval and deemed to present little difficulty in the overall approval process.

However, with the recent Appeal (AP 14-01 and AP 14-02) of the Conditional Approval, concern has again been raised regarding two issues of serious consequence to the residents of the Caufield Neighborhood Association: the plan to allow a cross-basin sewer connection to the Glen Oak drainage system and the continued presence of 26 parallel parking slots on the Beavercreek Road right-of-way as shown in the project's Site Plan.

Last night, at the scheduled meeting of the Neighborhood Association, these issues, among others, were again discussed. The Association voted unanimously to send a letter to you during this appeal period to emphasize two of our most pressing concerns.

The cross-basin connection, while granted as a Condition of Approval, will create potential sewer surcharging problems in a sensitive riparian zone along Glen Oak road from Coquille Drive to Highway 213. This will affect many homeowners and tenants of the townhomes along Glen Oak and adjacent arterials with the threat of sewer backup and overflow. There is also the risk of sewer flows reaching the waterways along Glen Oak Road. The proposed remedy--to enlarge the sewer lines in Highway 213--will lead to a cost to the City ratepayers/taxpayers. The project developers will only be paying 25% of the cost. The property owners in the Caufield Neighborhood will be facing 100% of the sewer backups. The current system is already under stress as described by the City itself. The sewer lines in Highway 213 will not need to be replaced immediately if this project does not go ahead. This will save citizens money for other more pressing repairs. And the cross-basin connection is not in keeping with the City's own master plan (SSMP) adopted only last year. We do not support the creation of a cross-basin connection into the Glen Oak basin to serve the Beavercreek Road Apartments.

Additionally, there was much concern about the 26 parallel parking spaces shown on the Site Plan for the project. Also at last night's meeting was Wes Rogers from the Oregon City School District Maintenance Department. He indicated that the District was also opposed to the parallel parking on Beaver Creek Road. It is understood that those parking spaces are not necessary to meet the required parking for the Project. However, we would like to ask that the elimination of the parallel parking on Beaver Creek Road be added as an additional condition to file SP 14-01.

Thank you for your time and consideration in this matter.

Sincerely,

Michael Mermelstein, Chairman

Caufield Neighborhood Association

From: [Aleta Froman-Goodrich](#)
To: [John M. Lewis](#)
Cc: [Todd Martinez](#); [Pete Walter](#)
Subject: Sewer Connection Beaver Creek Rd Apartments FW: Sewer Connection Questions
Date: Friday, January 23, 2015 4:21:16 PM

Hello John L,

I thought you should be aware of the following request from Mary Johnson to address questions, regarding the Beaver Creek Road Apartment project, Live/Work Development. The request is to answer the questions in her email below so that Mary can report back to the Caufield Neighborhood Association meeting on 1/27/2015

I will most likely not be able to address all of her questions by the 1/27 deadline but I will copy you on any responses I do send to the Mary Johnson.

For reference see Mary's email below. Please let me know if you have any comments that should be included in the response.

Thanks,
Aleta

From: Mary Johnson [mailto:amaryjohnson@hotmail.com]
Sent: Thursday, January 22, 2015 1:00 PM
To: Aleta Froman-Goodrich
Subject: Sewer Connection Questions

Dear Ms. Froman-Goodrich,

I have been asked to report back to the Caufield Neighborhood Association meeting next Tuesday with information regarding the Beaver Creek Road Apartment project, also known as the Oregon City Live/Work Development. I was at the City Council meeting last night to comment at the *de novo* hearing with my concerns about the cross-basin sewer connection for the apartment development. The options, as I understand them, are to allow the developers to pay a fee-in-lieu to connect either down Meyers Road to the Emerson Court sewer manhole or through the Emerson Court cul-de-sac using a public easement directly west of Beaver Creek Road.

My questions:

1. Would the City actually allow the brand-new Meyers Road pavement to be penetrated to allow a new sewer line?
2. Is there any consideration for the enormous disruption this project would create for the High School and local residents?
3. Is a fifteen-foot utility easement large enough to allow a sewer line extension from the development property, under or across Beaver Creek Road and down Emerson Court to the

existing sewer manhole?

4. Will this action set a precedent to other future developers who also might like to alleviate the costs of running over 5000 lineal feet of sewer line to their private development? and finally,

5. Will the new pavement overlay on Beavercreek Road also be penetrated with resultant patches and uneven travel surfaces?

Thank you for your time in answering these questions. The meeting for the Neighborhood Association is Tuesday, January 27

at 7:00 pm. I hope to have an answer from you before that, if possible. Thanks again.

Mary Johnson

971-678-7132

amaryjohnson@hotmail.com

From: [Brian Todd](#)
To: [Pete Walter](#)
Subject: Beaver creek Apt Project
Date: Sunday, February 01, 2015 10:04:53 PM

The Beaver creek Apartment Project is a concern to me because I live at 15034 Emerson Ct and I do not want the street in front of my house dug up. The culdesac in front of my house is on my property and I heard that to connect the sewer line they would have to dig it up. My wife and I take care of a special needs foster child that gets picked up for school everyday with a bus that can lift her and her wheelchair into the bus. I also think it is a terribly unsafe idea to put parking on the side of Beaver creek road. We have lived here for about 3 years and have already had two cats hit by cars on Beaver creek road. I also don't want the extra sewage going into the our sewers that may cause it to back up. Thank you for your time.

-Brian Todd

From: Pete Walter
To: ["Mike Fernandez"](#)
Subject: RE: Beaver Creek Apartment Project
Date: Monday, February 02, 2015 11:21:52 AM

Mr. Fernandez,

Your questions are important to me and the City and I wish to clear up any misconceptions, although we will be providing more background at the city commission meeting on February 18. Whether or not your comments are entered into the record at the hearing, we'll do our best to address your concerns.

With respect to sewer, the City Engineer Aleta Froman-Goodrich is the best contact. According to Aleta the pipe in Emerson is too shallow to meet engineering standards and the applicant will have to connect to Meyer's Road instead. This is consistent with Condition of Approval #34 of SP 14-01:

"34. The applicant shall provide 8-inch sanitary sewer collection system in the existing and future public right-of-way with the connection to the existing collection system at the manhole located in Meyers Road at Emerson Court. A short section of the collection system shall be located in a 15-foot wide public easement that extends from the western end of "B" Street to the Beaver Creek Road ROW."

I am not sure how to respond to your general statement regarding the medical needs of seven families, although I sympathize with the individuals involved and their needs. We collaborate very closely with the police, fire district and school district when we review developments. None of the agencies involved indicated a conflict with the development when we transmitted it for review, although the school district did indicate a preference for no parking on Beaver Creek Road, which has been accommodated in the findings and conditions of approval.

The 15' wide public pedestrian access way to Emerson Court is a Public Utility Easement as well as a Public Access Easement. This would likely not be affected permanently, if at all.

As always, a complete staff report responding to all of these concerns will be available prior to the hearing on February 18. Staff won't be responding to testimony on February 4.

Pete Walter

From: Mike Fernandez [mailto:cheeseandwine62@gmail.com]
Sent: Monday, February 02, 2015 10:15 AM
To: Pete Walter
Subject: Re: Beaver Creek Apartment Project

Thank you Mr. Walter for responding so quickly. The meeting on the 27th was the first I

learned about the possibility of a sewer line going down in front of my home. If it was discussed at an earlier meeting, due to my wife's medical needs, I was unable to attend. I think two issues the Commission must be award of:

1. The alley from Beaver Creek Rd. onto Emerson Ct. is private and owned by 7 residences, and
2. the medical needs of five families.

Sincerely,
Mr. Fernandez

On Mon, Feb 2, 2015 at 9:24 AM, Pete Walter <pwalter@ci.oregon-city.or.us> wrote:

Dear Mr. Fernandez,

Thank you. I will forward your comments along to the City Commission. Please be advised that since you did not comment in writing during either of the two initial public comment periods on SP 14-01, the City Commission has discretion on whether or not to include these comments in the record for the appeal.

Pete Walter

From: Mike Fernandez [mailto:cheeseandwine62@gmail.com]

Sent: Sunday, February 01, 2015 11:22 AM

To: Pete Walter

Subject: Beaver Creek Apartment Project

Dear Pete Walter

I attended the Caulfield Neighborhood meeting on the 27th and learned about the apartment project SP 14-01 across my street. I live on Emerson Ct.

There are several matters my family and I do not support:

1. The "cross-basin" connection sewer line is planned to go in front of my house; we would not be able to use the garage, because the alley is about as wide as the length of a car. The alley between Beaver Creek Rd. and Emerson Court is private, owned by myself and six other residences, who would also be denied access to their homes. The added 183 units would also overtax our neighborhood sewer system. I am concerned about the back flow.
2. The traffic along 2-lane Beaver Creek Road is always a problem during rush hours and before and after the high school hours. Allowing parallel parking slots along the road would create more problems as well as safety issues. The high school and elementary buses also converge at that intersection.

3. There is planned for only one exit and entrance road into the apartments, with estimated of 343 parking spots in the complex. That seems very unsafe and traffic problems at the Meyers Rd. and Beavercreek Rd. intersection. Within a year, the new school bus complex will be completed at the end of Meyers Rd., behind the high school. It is estimated that many of the 80-100 buses will be using the 2-lane Meyers Rd., at least twice a day. There is already backed up traffic with school buses, parents picking up their children, high school students and staff, as well as residents in the two courts off Meyers Rd. and those living in The Landing apartment buildings every day at that intersection.

4. As a resident of Oregon City, I do not appreciate the idea that the Beavercreek Apartment Project is only paying 25% to improve the sewer lines and the rest of the community will have to pay higher taxes for the remaining 75%! Without the new apartment units, the sewer lines would not have to be upgraded and avoid "surcharging" the Glen Oak system, which could cause sewer backups to downstream residences.

5. Impact on the neighborhood if Meyers Rd. and Beavercreek Rd are dug up for the sewer lines:

(a) those living on the alley

-One family has 6-month old twins. She would have to walk down the street in kinds of weather with them + her and their belongings.

-One family has an artistic child.

-One family has a special need child in a wheelchair. Her school picks her up every day in front of her house.

(b) those living on Emerson Ct.

-One woman walks with a cane and is in her 90s. She would not be able to pull into her garage with her groceries and walk that far.

-One family has two children with medical problems and goes to the emergency room often.

-The elementary school bus drops off the children on Beavercreek Rd., and their parents walk them down the alley to their homes.

-There is a police officer and a warden at the Woman's Corrections who are both on call.

-There are 12 elementary children who would not be able to play outside nor visit one another, safely.

(c.) public

-Many high school students park their cars all day in the court.

-Up to ten or more parents pick up their high school students while parking on Emerson.

-During high school's after and evening programs, Emerson Ct. is filled with all the overflow cars.

-At least three times a week a UPS truck delivers on Emerson.

-The weekly three sanitation trucks are up and down Emerson, as well as back onto the alley.

-More than 35 residential cars would try to park on the street during any sewer constructions.

-The residential mailboxes are located on Emerson at the corner of Meyers Rd.

-Finally, it would be devastating if a firetruck or ambulance is needed and the court is filled with park cars and a torn up street!

Thank you for listening and presenting this at the February 4th meeting,

Mr. and Mrs. Mike Fernandez

15028 Emerson Ct.

[503-5182450](tel:503-5182450)

Please include this into the record for SP 14.01, Continuation Hearing for the proposed 180-Unit Apartment Complex on Beaver Creek Road

In the Oregonian 1/25/15, within the Metro Section - page A-10 is an article on a 180-unit apartment complex that is being proposed on 9.7 acres. It is just under the 10-acre requirement for a master plan, where all criteria are adjudicated, including all transportation considerations. This location is somewhat across the street from this combination of Oregon City High School and Clackamas Community College on Beaver Creek Road.

The critical needed infrastructure in this area we all know is limited, in virtually every aspect.

There is not a currently a passed and approved Concept Plan for the Beaver Creek Concept Plan Area. Therefore there are NO authenticated studies that validate needs assessments, of what the infrastructure needs that have to be in place, for virtually any change in usage to water, sewer and transportation, with new development. There are also not the needed understanding on how to collectively fund what might be needed or yet to be identified in permanent new infrastructure.

Much of this area is not serviced by Oregon City Water in combination with Tri-Cities Water Service District. Currently this apartment site and much of this area gets its water from Clackamas River Water District within a Inter Governmental Agreement (IGA). Oregon City and Tri-Cities Water District needs to develop their own adequate permanent water supply infrastructure to meet the needs of any development that is being advanced along Beaver Creek Road, let alone these proposed apartments. The financial side of creating that needed permanent infrastructure in this area has not been identified.

This Beaver Creek Road area does not have the needed/adequate Sanitary Sewer Infrastructure and it would require maybe \$2-to \$4-Million Plus too create adequate infrastructure to service all of the proposed economic development initiatives, new housing, as well as these apartments. The financial side of creating the needed/required permanent infrastructure in this area has not been identified and exceeds what is currently planned within Sewer Rate increases in Fees. This is outside - exceeds those funding understandings.

Beaver Creek Road is one lane in each direction immediately past the critical interchange with State Highway 213, going east. There are many choke points in this area that restrict traffic flow and equally have negative impacts on safety that exist on Beaver Creek Road. Within the criteria of the accepted ITE Transportation Manuals this area is currently achieving Level of Service ratings of "D and F" within the AM and PM Peak Periods, which exceeds what is necessary to advance virtually any new development that has significant new increases in "Incidents of Travel" being generated. Having the LOS rating of "D and F" means that Beaver Creek Road is in failure right now, F is as bad as you get.

ENTERED INTO THE RECORD

DATE RECEIVED: 2/4/15

Item 6b. SUBMITTED BY: Paul Edgar

A very flawed beginning level studies of transportation needs for this area even suggested expanding and creating a major arterial alternative within the Holly Lane Right-of-Way (ROW) that would create a bypass to Redland Road. Holly Lane is in a major known landslide area, with active landslides going on all of time. Redland Road is equally inadequate and in a Geologic Hazard area within wetlands and virtually cannot be reasonably expanded in capacity. Any engineer would validate that it is not responsible to even consider any effort to expand and create the needed capacity and improvements that would be needed/required, to make Holly Lane into Beaver Creek Road - Bypass Arterial.

The only way to create the needed Transportation Capacity and Infrastructure is to build out Beaver Creek Road to a total of 5-lanes, two in each direction with a center turn lane, all of the way east past Holly Lane. Without doing this virtually all of the 6,000-plus households from an area east of the Beaver Creek Road and State Highway-213 Interchange that connect to State Highway-213 would be effectively land locked and capable of entering into a Lawsuit against Oregon City. This exposure would be costly and is unnecessary and would delay and detract from all other critical activities and plans.

The City of Oregon City cannot advance this proposed 180-unit Apartment Complex and any economic development plan along Beaver Creek Road until it has an approved Beaver Creek Road Concept Plan and with it the necessary funding to create the needed infrastructure. If anything gets advance without financial side of these considerations completely identified, it might therefore put the full burden of the cost where it could fall on to the backs of all households and businesses in Oregon City and surrounding area with unnecessary negative consequences.

Paul Edgar was a member to Clackamas County Transportation Committee the developed recent CC Transportation Systems Plan (TSP) and lives in Oregon City.

From: [Carrie Richter](#)
To: [Pete Walter](#); [Tony Konkol](#)
Subject: FW: Feb. 4 Hearing
Date: Wednesday, January 28, 2015 10:52:47 AM
Attachments: [image001.png](#)

FYI.

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► land use | condemnation | real estate e-forum: www.northwestlandlawforum.com

From: Steven Hultberg [mailto:SHultberg@radlerwhite.com]
Sent: Wednesday, January 28, 2015 10:20 AM
To: Carrie Richter
Subject: RE: Feb. 4 Hearing

Carrie,

This email confirms that on behalf of the applicant we are extending the 120-day limit until March 4, 2015.

Please let me know if you need anything further.



Steven P. Hultberg

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